



TRANSPORTATION CABINET

Frankfort, Kentucky 40622
www.transportation.ky.gov/

Steven L. Beshear
Governor

Michael W. Hancock, P.E.
Secretary

November 14, 2012

CALL NO. 311
CONTRACT ID NO. 121056
ADDENDUM # 2

Subject: McCracken County, FD04 SPP 073 0024 003-017
Letting November 16, 2012

- (1) Revised - Special Notes - Pages 137-139(a) of 234
- (2) Revised - Summary - Page 163 of 234
- (3) Revised - Typical Section & Detail Sheet - Pages 163(a)-163(b) of 234
- (4) Revised - Detail Sheet - Page 184 of 234
- (5) Revised - Typical Section & Expansion Joint - Pages 200-201 of 234
- (6) Revised - Bid Items - Pages 231-234 of 234

Proposal revisions are available at <http://transportation.ky.gov/Construction-Procurement/>.

If you have any questions, please contact us at 502-564-3500.

Sincerely,

A handwritten signature in blue ink that reads "Ryan Griffith".

Ryan Griffith
Director
Division of Construction Procurement

RG:ks
Enclosures



An Equal Opportunity Employer M/F/D

SPECIAL NOTE FOR BEARING REPLACEMENT

I. DESCRIPTION

Perform all work in accordance with the Kentucky Transportation Cabinet, Department of Highway's 2012 Standard Specifications for Road and Bridge Construction and applicable Supplemental Specifications, the Standard Drawings, this Note, and attached detail drawings. Section references are to the Standard Specifications.

This work consists of the following:

- (1) Furnish all labor, materials, tools, and equipment.
- (2) Jack and temporarily support the specified girders in the attached detail drawings for bridges 073B00105L and 073B00114R.
- (3) Remove existing rocker bearing assemblies (and shim plates when present) as specified in this note and in accordance with the details.
- (4) Install new elastomeric bearings and assemblies as specified in this note and in accordance with the plans.
- (5) Maintain and control traffic
- (6) Any other work specified as part of this contract.

II. MATERIALS

- A. Structural Steel.** Use AASHTO M270 (ASTM A709) Grade 50 steel suitable for welding.
- B. Weld Material.** See Section 813.10. All welds shall be E70XX.
- C. Elastomeric Bearing Pad.** See Section 822. Pads shall be low temperature Grade 3 with durometer hardness of 50 and shall be subjected to the load testing requirements corresponding to Design Method B.
- D. Paint.** Match the color of the existing paint. All paint supplied must be contained in the current List of Approved Materials. See Section 821.
- E. Bolts, Nuts, and Washers.** All bolted connections are AASHTO M164 (ASTM A325) high strength bolts, nuts, and washers.

III. CONSTRUCTION

- A. Existing Plans.** As an aid to the Contractor, plans of the existing bridges are available from the Division of Structural Design upon request. The completeness of the drawings is not guaranteed and no responsibility is assumed by the Kentucky Transportation Cabinet for their accuracy. The existing drawing numbers for the bridges with bearing replacements are as follows:

<u>Crossing</u>	<u>Maintenance #</u>	<u>Drawing #</u>
I-24 over US 60	073B00105L	17867
I-24 over Pool Road, etc.	073B00114R	18700

- B. Existing Dimensions.** The Contractor shall verify all dimensions with field measurements prior to ordering materials or fabricating steel.
- C. Shop Plans.** Shop plans will not be required. The Contractor is responsible for obtaining field measurements and supplying properly sized materials to complete the

work. New material that is unsuitable because of variations in the existing structure shall be replaced at the Contractor's expense.

- D. Sequence of Construction.** Bridge end jacking and replacement of the specified bearing assemblies at the east end of bridge # 073B00114R shall not be performed until after all joint replacement and approach slab work has been completed on that bridge.
- E. Jacking Plan.** The Contractor must submit a jacking plan for review prior to starting work. The design must be stamped by a professional engineer licensed in the State of Kentucky. The girders shall only be lifted enough to allow for removal and installation of the bearing assemblies, and no more than 1/4". To prevent deck cracking, jack all five girders at each support concurrently and limit differential movement between girders lines to 1/8". For each girder the total estimated design loads to be supported are:

<u># 073B00105L (Both Abut.)</u>	<u># 073B00114R (East End Bent)</u>
Dead Load 42 kip	Dead Load 50 kip
<u>Live Load 85 kip</u>	<u>Live Load 88 kip</u>
Total 127 kip	Total 138 kip

The Contractor's jacking system shall be designed to support a minimum of 200% of these loads. Before jacking operations begin, temporary stiffener angles for the girder webs must be installed above the jack locations as shown in the attached detail drawings. Jacking and supporting the steel girders is incidental to the contract unit price for "Bearing Replacement".

- F. Metal Railing and Lighting Conduit.** On Bridge 073B00105L, metal railing and lighting conduit in the barrier is continuous across the expansion joints the bridge ends. The Contractor shall take all necessary precautions to prevent damage to these components during jacking operations. The Contractor and Engineer shall inspect and note their condition before and after jacking. Should damage have occurred during jacking, the Contractor shall make all necessary repairs as directed and approved by the Engineer at no additional cost to the Department.
- G. Maintenance of Traffic.** Maintain and control traffic in accordance with the Standard Specifications and the Traffic Control Plan for this project.
- H. Remove Existing Material.** Remove the existing shim plates and rocker bearing assemblies (including bolts) as shown in the attached detail drawings. Dispose of all removed material complete away from the job site. This work is incidental to the contract unit price for "Bearing Replacement".
- I. Concrete Patching Repair.** Remove all debris and loose concrete from the concrete pedestals. Patch all cracks and delaminated concrete prior to replacing the bearings to provide a smooth and level surface for the elastomeric bearing pad. This work shall be incidental to "Concrete Patching Repair". See separate special note.
- J. Field Prepare Existing Surfaces.** Existing surfaces of the girders to be welded to or in contact with new steel shall be cleaned of all corrosion, debris, and deleterious substances before welding or installation of new steel. The surface between the bottom flange of the girders and the bearing plate shall be made reasonable true and flat by grinding or other method approved by the Engineer, to provide a uniform bearing surface. This work is incidental to the contract unit price for "Bearing Replacement".
- K. Install New Bearing Assembly.** Install the new bearing assemblies as shown in the attached detail drawings. See the attached detail drawings for information regarding the height and alignment of the new bearing assemblies. Protect the elastomeric pad from heat during field welding. The bearing assembly must meet the fabrication, testing, and

installation requirements of the AASHTO Standard Specification, Division II, Section 18.

- L. Welding Specifications.** All welding and welding materials shall conform to Joint Specifications ANSI/AASHTO/AWS D1.5M-D1.5-2008 Bridge Welding Code. Modifications and additions as stated on the detail drawings shall supersede the ANSI/AASHTO/AWS specification. Nondestructive testing by the contractor (QC) will not be required. Welding procedures shall be submitted to the Engineer and approved prior to the start of fabrication and retrofit. The cost of welding, welding materials, straightening, altering, and burning new or existing steel shall be included in the contract unit price for “Bearing Replacement”.
- M. Prohibited Field Welding.** Except as shown on the plans, no welding of any nature shall be performed on the bridge without written consent of the Director, Division of Structural Design, or an authorized representative, and then only in the manner and at the locations designated in the authorization.
- N. Mill Test Reports.** Notarized test reports shall be furnished in triplicate to the Department showing that all the materials used for these repairs conform to the requirements of the Specifications.
- O. Painting.** All existing faying surfaces where new steel is to be installed and any areas of existing paint damaged from replacement of joints or removal of existing bearing assemblies shall be cleaned and receive prime coat of the selected coating system as specified in the section 607.03.23 of Standard Specification before any new steel is installed. Level of cleaning shall be to an SSPC-SP 15 (Commercial Grade Power Tool Cleaning). All Power tools shall be equipped with vacuum shrouds and fitted with HEPA filters at their air exhausts. Maintain and operate all vacuum shrouded power tools to collect generated debris. All new structural steel shall be cleaned and painted as specified in Section 607.03.23 of Standard Specification. Contrary to Section 607.03.23 of Standard Specification, all coats of the selected coating system shall be shop applied. Necessary touch up/repair of the shop applied paint system on the new steel may be performed in the field. Necessary touch up/repair of existing paint damaged on non faying surfaces of the existing steel from cleaning and any existing paint damaged from the replacement of joints, including cut end of girder(s), or removal of existing bearing assemblies shall receive the remaining coats of the selected coating system as specified in 607.02.23 of the Standard Specification. The finish coat shall be gray closely approaching Federal Standard 595 Color FS X6187. Residual lead paint may still be on bridge. The Contractor is advised to take all necessary protective measures including worker safety and environmental regulations when performing surface preparation. The Department will not consider any claims based on residual lead paint. Cleaning and painting of all new and existing structural steel will be considered incidental to the contract unit price for “Bearing Replacement”.
- P. Damage to the Structure.** The Contractor shall bearing full responsibility and expense for any and all damage to the structure during the repair and retrofit work; even to the removal and replacement of fallen spans, should the damage result from the Contractor’s actions.

IV. MEASUREMENT

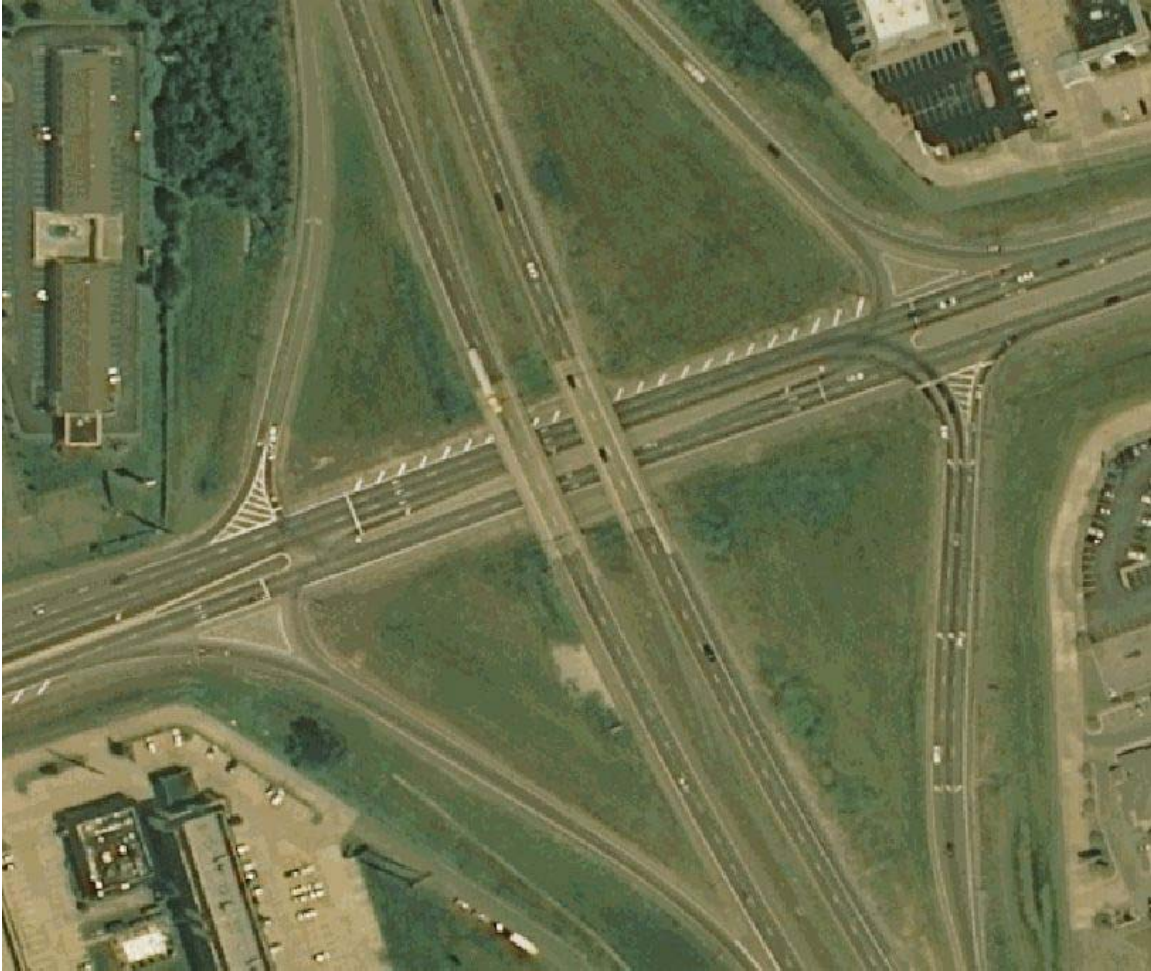
- A. Bearing Replacement.** Measurement will be for each girder bearing that is removed and replaced.

V. **PAYMENT**

- A. Bearing Replacement.** Payment at the contract unit price is full compensation for (1) jacking and temporarily supporting the ends of the steel girders, (2) removing and disposing of the existing shim plates and rocker bearings assemblies (3) preparing the interface surfaces for welding and painting, (4) furnishing and installing new bearing assemblies (including elastomeric bearing pads), (5) painting the steel surfaces as specified in this note, and (6) all other materials, labor, equipment, tools, and incidentals necessary to complete the work as specified by this note.

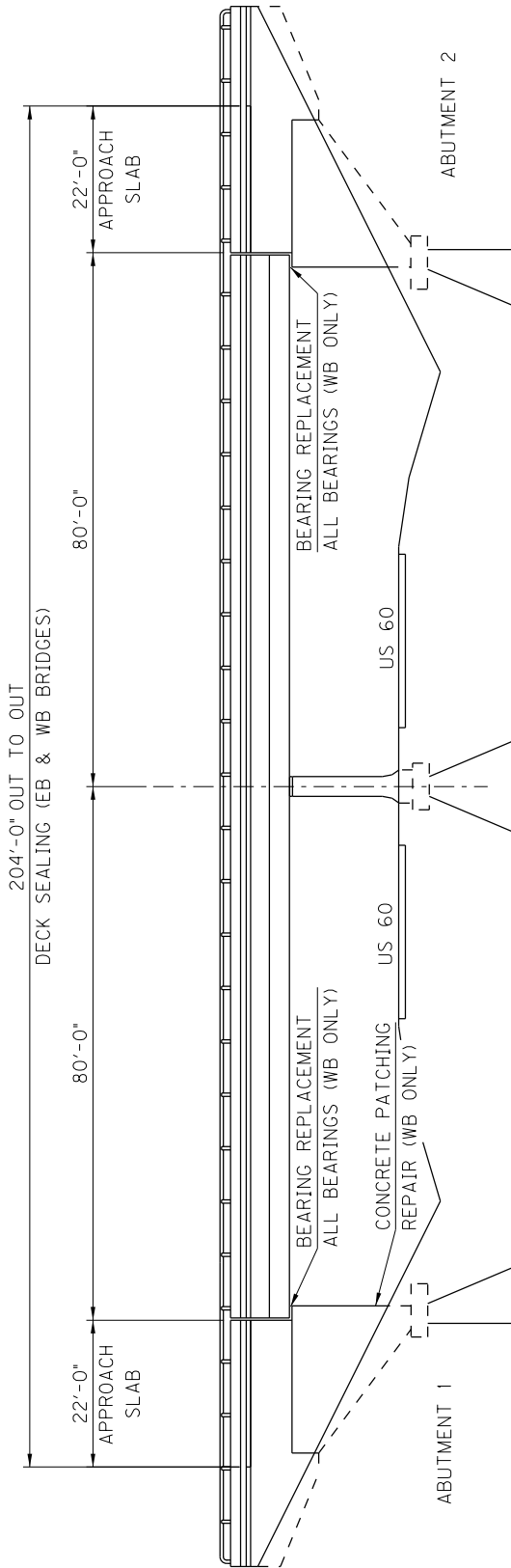
The Department will consider payment as full compensation for all work required by this note and the detail drawings.

I-24 OVER US 60 (073B00105L&R)
(MP 4.3)



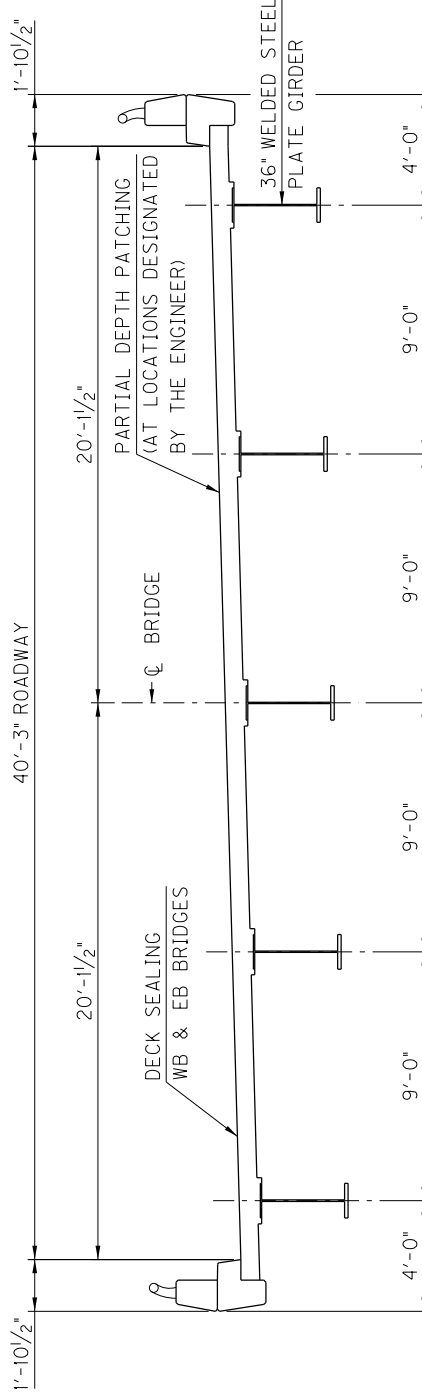
<u>SUMMARY OF QUANTITIES</u>			
ITEM CODE	DESCRIPTION	QUANTITY	UNIT
21969NN	BEARING REPLACEMENT	10	EACH
22146EN	CONCRETE PATCHING REPAIR	50	SQFT
24094EC	PARTIAL DEPTH PATCHING	1	CY
24106EC	DECK SEALING	17950	SQFT

NOTE #1: PERFORM PARTIAL DEPTH PATCHING OF BRIDGE DECKS AT LOCATION(S)
DESIGNATED BY THE ENGINEER.



ELEVATION
 NTS

NOTE: LIGHTING CONDUIT IS
 PRESENT IN FASCIA BARRIER
 OF EB AND WB BRIDGES



TYPICAL SECTION (MAIN SPANS)
 NTS

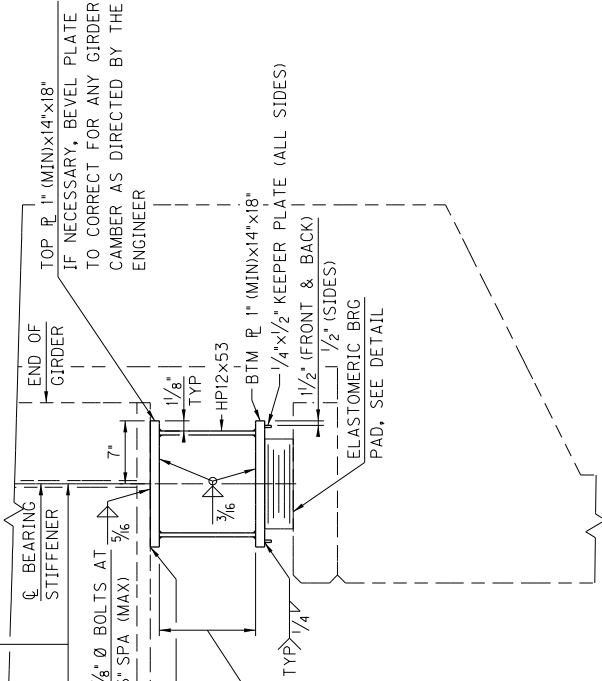
NOTE:
 ALL DIMENSIONS ON THESE DETAIL
 SHEETS THAT REFER TO THE EXISTING
 STRUCTURE ARE FROM EXISTING
 PLANS AND ARE APPROXIMATE.

COUNTY: MCCRACKEN
 ROUTE: 1-24
 CROSSING: US 60

ELEVATION AND TYPICAL SECTION



ADJUST AS NEEDED TO LOCATE STIFFENER DIRECTLY ABOVE JACK. DISTANCE FROM \bar{C} SHALL NOT EXCEED 3'-0".



L6x4x $\frac{7}{8}$ TEMP BRG STIFFENER EACH SIDE

COPE ANGLE FOR TIGHT FIT AGAINST TOP AND BOTTOM FLANGE

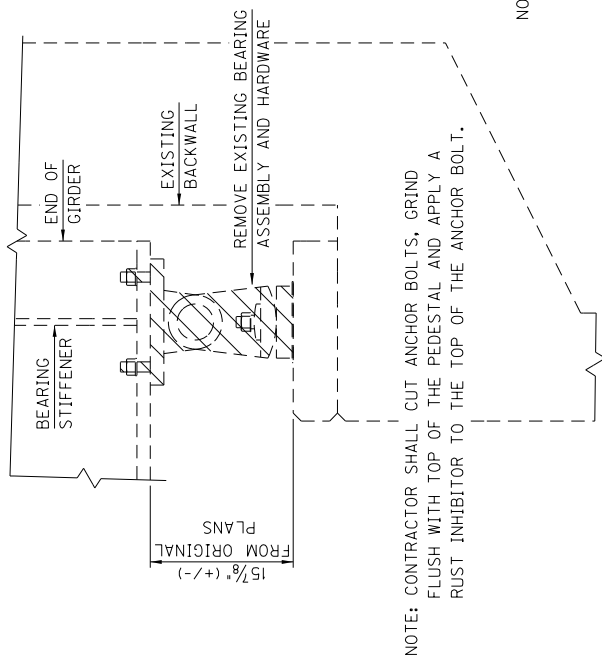
CAULK ANY GAPS BETWEEN BACK FACE AND BEARING PLATE PRIOR TO PAINTING, FRONT & BACK

CONTRACTOR SHALL VERIFY HEIGHT OF HP12x53 NEEDED AT EACH BEARING TO MAINTAIN PROPER VERTICAL ALIGNMENT OF EXPANSION JOINT ABOVE. ADD $\frac{1}{16}$ " TO THE FIELD MEASUREMENT FOR COMPRESSION OF BEARING PAD

NOTE: CONTRACTOR SHALL REMOVE ANY DEBRIS AND LOOSE CONCRETE, PATCH ALL CRACKS AND DELAMINATED CONCRETE PRIOR TO REPLACING BEARING. COST IS INCIDENTAL TO "CONCRETE PATCHING REPAIR."

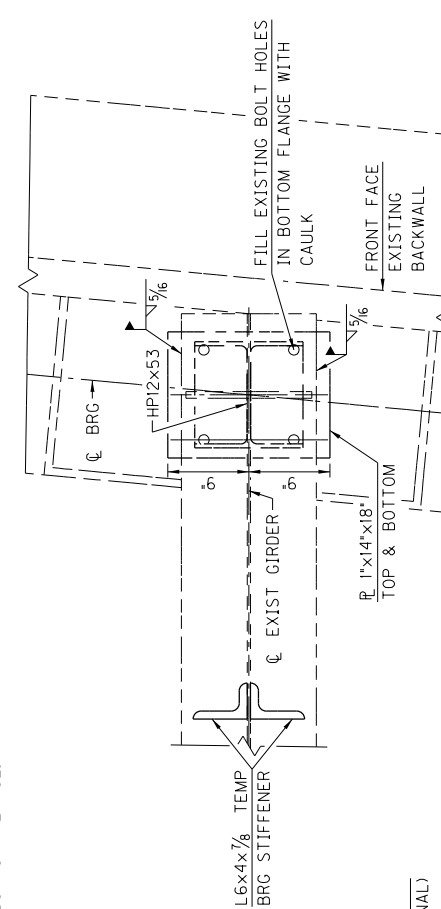
TEMPORARY BEARING STIFFENERS SHALL BE FULL HEIGHT OF WEB, EXCEPT UNDER LONGITUDINAL STIFFENERS USE 12" HEIGHT FLUSH AGAINST BOTTOM FLANGE.

REPLACEMENT BEARING



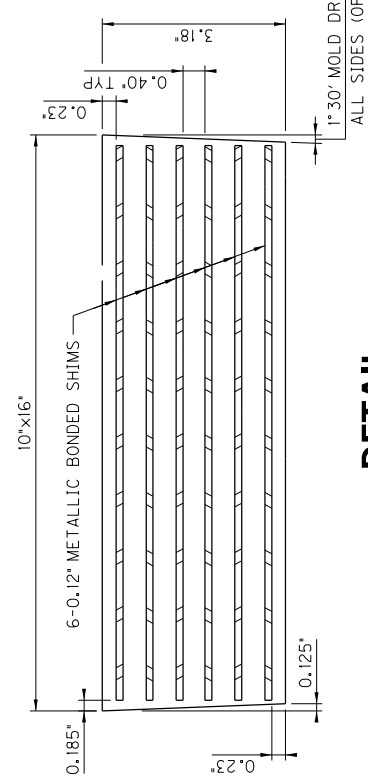
EXISTING BEARING

(SHOWING REMOVAL)



PLAN

(SHOWING REPLACEMENT BEARING)



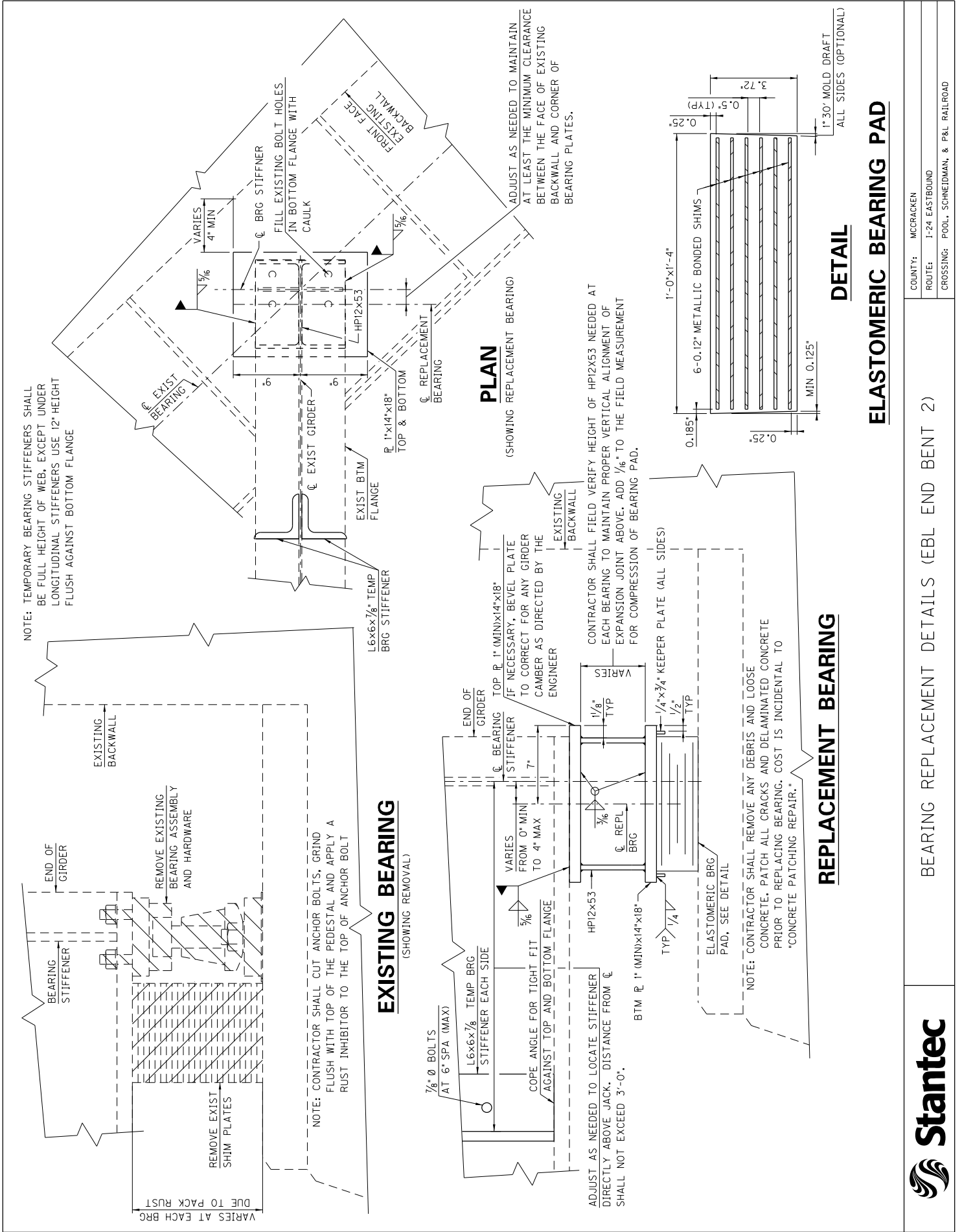
DETAIL

ELASTOMERIC BEARING PAD

BEARING REPLACEMENT DETAILS

COUNTY: MCCRACKEN
ROUTE: 1-24
CROSSING: US 60





NOTE: TEMPORARY BEARING STIFFENERS SHALL BE FULL HEIGHT OF WEB, EXCEPT UNDER LONGITUDINAL STIFFENERS USE 12" HEIGHT FLUSH AGAINST BOTTOM FLANGE

NOTE: CONTRACTOR SHALL CUT ANCHOR BOLTS, GRIND FLUSH WITH TOP OF THE PEDESTAL AND APPLY A RUST INHIBITOR TO THE TOP OF ANCHOR BOLT

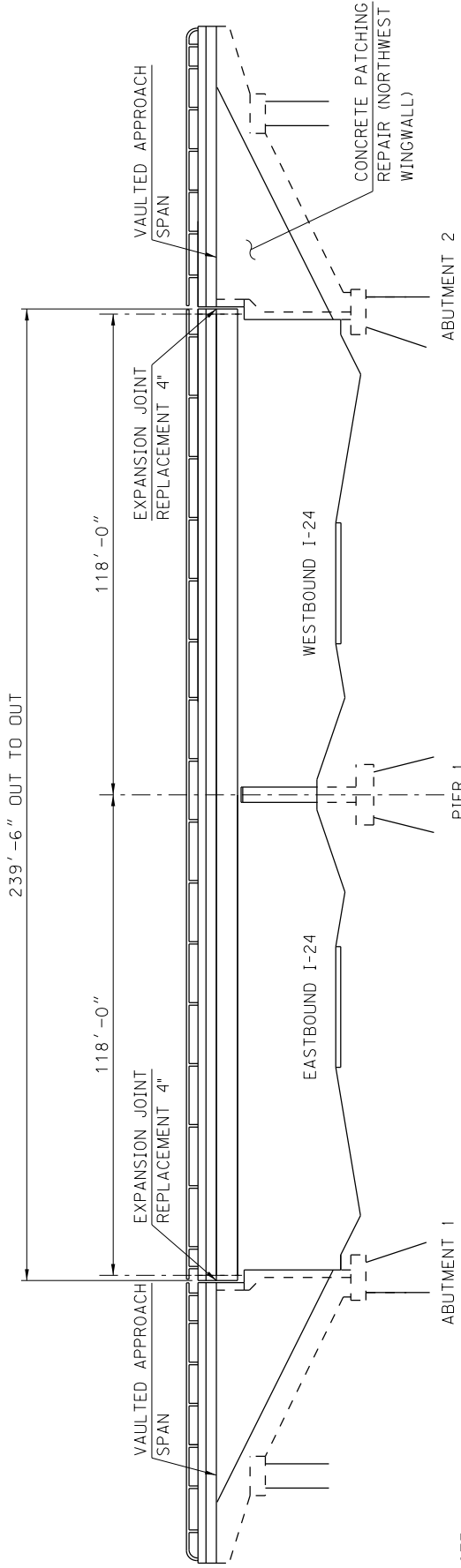
ADJUST AS NEEDED TO MAINTAIN AT LEAST THE MINIMUM CLEARANCE BETWEEN THE FACE OF EXISTING BACKWALL AND CORNER OF BEARING PLATES.

CONTRACTOR SHALL FIELD VERIFY HEIGHT OF HPI2X53 NEEDED AT EACH BEARING TO MAINTAIN PROPER VERTICAL ALIGNMENT OF EXPANSION JOINT ABOVE. ADD 1/16" TO THE FIELD MEASUREMENT FOR COMPRESSION OF BEARING PAD.

NOTE: CONTRACTOR SHALL REMOVE ANY DEBRIS AND LOOSE CONCRETE. PATCH ALL CRACKS AND DELAMINATED CONCRETE PRIOR TO REPLACING BEARING. COST IS INCIDENTAL TO "CONCRETE PATCHING REPAIR."



COUNTY: MCCRACKEN
ROUTE: I-24 EASTBOUND
CROSSING: POOL, SCHNEIDMAN, & P&L RAILROAD



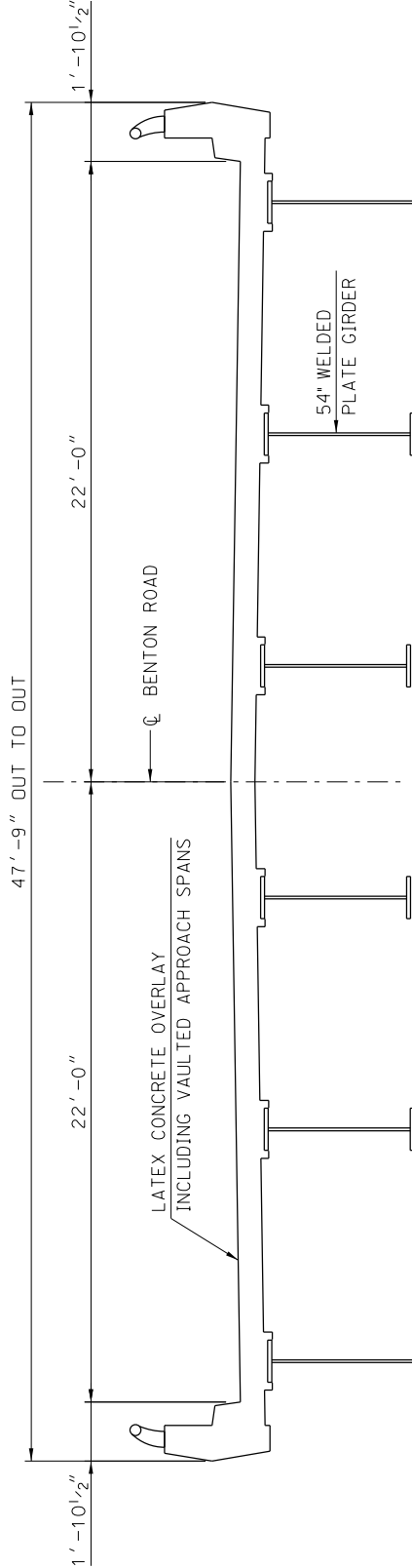
ELEVATION

39°12'0\"/>

NTS

NOTE:

AT THE VAULTED APPROACH SPANS, SET THE FINISHING MACHINE RAILS TO PROVIDE THE TRANSITION FROM THE JOINT AT THE BRIDGE ABUTMENT TO THE EXISTING ARMORED EDGE AT THE END OF THE APPROACH SPANS. THE TRANSITION SHOULD MATCH THE FINISHED GRADE OF THE OVERLAY WITH THE EXISTING GRADE OF THE ADJACENT PAVEMENT. REMOVE THE EXISTING CONCRETE AS NECESSARY TO MAINTAIN THE MINIMUM SPECIFIED THICKNESS OF THE OVERLAY.



TYPICAL SECTION

NTS

NOTE:
 ALL DIMENSIONS ON THESE DETAIL SHEETS THAT REFER TO THE EXISTING STRUCTURE ARE FROM THE EXISTING PLANS AND ARE APPROXIMATE.

ALL REPAIRS TYPICAL FOR NB & SB BRIDGES, UNLESS NOTED OTHERWISE

COUNTY:	MCCRACKEN
ROUTE:	BENTON ROAD (KY 131)
CROSSING:	I-24

ELEVATION AND TYPICAL SECTION



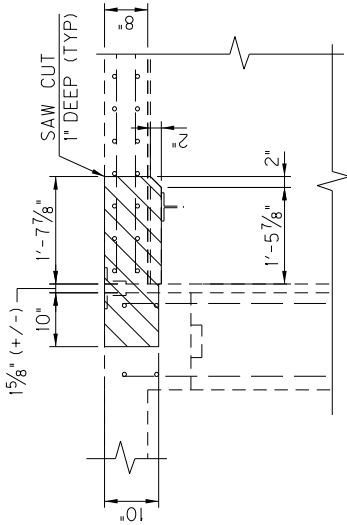
NOTES:

REMOVE HATCHED AREAS OF CONCRETE, EXPANSION DEVICE AND ARMORED EDGE. CLEAN, STRAIGHTEN & REUSE EXISTING REINFORCEMENT. THE CONTRACTOR HAS THE OPTION TO REPLACE EXISTING TRANSVERSE REINFORCEMENT.

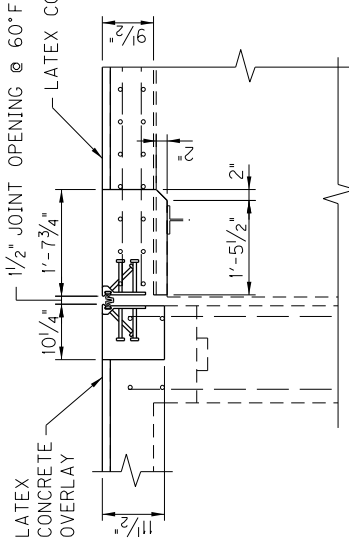
FOR TEMPERATURE CHANGE INCREMENTS, SEE STD DWG BJE-001-12 PERFORMED EXPANSION JOINT STRIP SEAL SHALL BE EITHER ONE OF THE FOLLOWING OR AN APPROVED EQUIVALENT:

D.S. BROWN COMPANY
 L2-400 WITH SSPA STEELFLEX RAIL

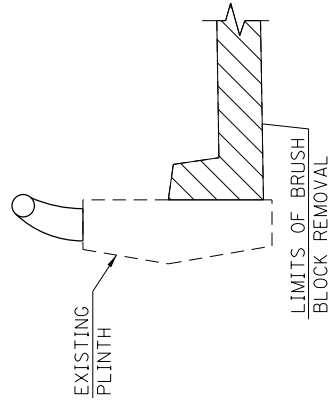
WATSON BOWMAN ACME
 SE-400 WITH TYPE "P" RAIL



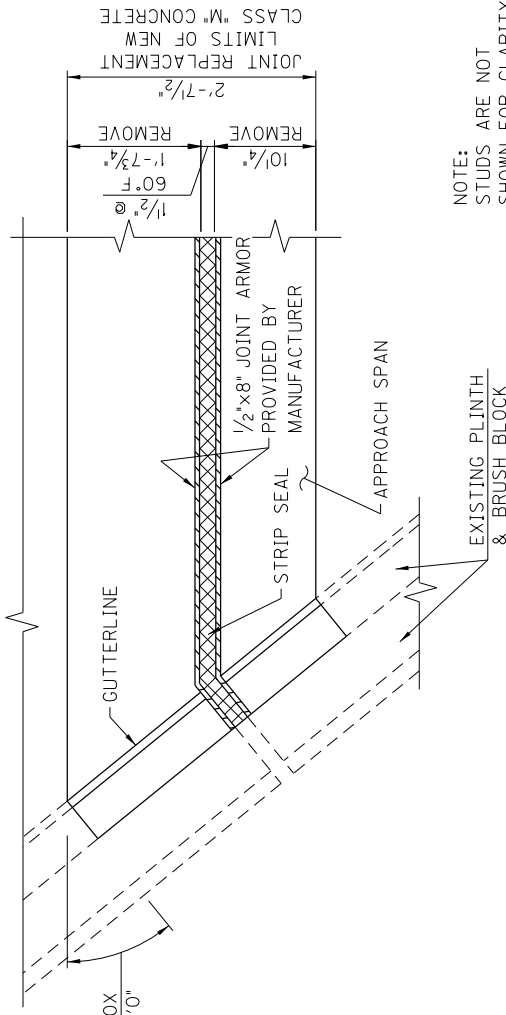
EXISTING SECTION
 (SHOWING REMOVAL)



PROPOSED SECTION
 (SHOWING REMOVAL)

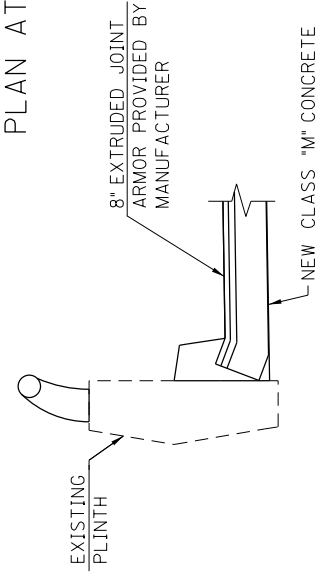


EXISTING SECTION
 AT BARRIER
 (SHOWING REMOVAL)



PLAN AT NORTH ABUTMENT

NOTE:
 STUDS ARE NOT
 SHOWN FOR CLARITY



PROPOSED SECTION
 AT BARRIER

NOTE:
 ALL DIMENSIONS ON THESE DETAIL SHEETS THAT REFER TO THE EXISTING STRUCTURE ARE FROM THE EXISTING PLANS AND ARE APPROXIMATE.

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PROPOSAL BID ITEMS

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Section: 0001 - PAVING

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
0010	00001		DGA BASE	7,700.00	TON		\$	
0020	00100		ASPHALT SEAL AGGREGATE	3,212.00	TON		\$	
0030	00103		ASPHALT SEAL COAT	386.00	TON		\$	
0040	00312		CL3 ASPH SURF 0.50D PG64-22	10,155.00	TON		\$	
0050	00335		CL4 ASPH SURF 0.50A PG76-22	37,360.00	TON		\$	
0060	02081		JPC PAVEMENT-8 IN SHLD	34.00	SQYD		\$	
0070	02677		ASPHALT PAVE MILLING & TEXTURING	47,515.00	TON		\$	
0080	20071EC		JOINT ADHESIVE	241,245.00	LF		\$	
0090	20757ED		PAVEMENT REPAIRMAJOR REPAIR	72.00	SQYD		\$	
0100	20757ED		PAVEMENT REPAIRMINOR REPAIR	350.00	SQYD		\$	

Section: 0002 - ROADWAY

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
0110	00078		CRUSHED AGGREGATE SIZE NO 2	31.00	TON		\$	
0120	01845		ISLAND INTEGRAL CURB	425.00	LF		\$	
0130	01877		SPECIAL HEADER CURB	35,271.00	LF		\$	
0140	01891		ISLAND HEADER CURB TYPE 2	1,225.00	LF		\$	
0150	01904		REMOVE CURB	36,496.00	LF		\$	
0160	01982		DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL WHITE	548.00	EACH		\$	
0170	01983		DELINEATOR FOR GUARDRAIL MONO DIRECTIONAL YELLOW	135.00	EACH		\$	
0180	01984		DELINEATOR FOR BARRIER - WHITE	65.00	EACH		\$	
0190	01985		DELINEATOR FOR BARRIER - YELLOW	65.00	EACH		\$	
0200	02200		ROADWAY EXCAVATION	10.00	CUYD		\$	
0210	02220		FLOWABLE FILL	48.00	CUYD		\$	
0220	02237		DITCHING	70,000.00	LF		\$	
0230	02273		FENCE-4 FT CHAIN LINK	10.00	LF		\$	
0240	02352		GUARDRAIL-STEEL W BEAM-D FACE	3,437.50	LF		\$	
0250	02363		GUARDRAIL CONNECTOR TO BRIDGE END TY A	51.00	EACH		\$	
0260	02365		CRASH CUSHION TYPE IX-A	24.00	EACH		\$	
0270	02367		GUARDRAIL END TREATMENT TYPE 1	11.00	EACH		\$	
0280	02369		GUARDRAIL END TREATMENT TYPE 2A	45.00	EACH		\$	
0290	02381		REMOVE GUARDRAIL	58,275.00	LF		\$	
0300	02387		GUARDRAIL CONNECTOR TO BRIDGE END TY A-1	25.00	EACH		\$	
0310	02391		GUARDRAIL END TREATMENT TYPE 4A	33.00	EACH		\$	
0320	02483		CHANNEL LINING CLASS II	1,832.00	TON		\$	
0330	02484		CHANNEL LINING CLASS III	6,719.00	TON		\$	
0340	02562		SIGNS	2,000.00	SQFT		\$	
0350	02599		FABRIC-GEOTEXTILE TYPE IV	7,809.00	SQYD		\$	
0360	02650		MAINTAIN & CONTROL TRAFFIC	1.00	LS		\$	
0370	02671		PORTABLE CHANGEABLE MESSAGE SIGN	10.00	EACH		\$	
0380	02676		MOBILIZATION FOR MILL & TEXT	1.00	LS		\$	
0390	02696		SHOULDER RUMBLE STRIPS-SAWED	245,816.00	LF		\$	
0400	02714		SHOULDERING	60,000.00	LF		\$	

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PROPOSAL BID ITEMS

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LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
0410	02775		ARROW PANEL	6.00	EACH		\$	
0420	02929		CRASH CUSHION TYPE IX	12.00	EACH		\$	
0430	05950		EROSION CONTROL BLANKET	66,720.00	SQYD		\$	
0440	06407		SBM ALUM SHEET SIGNS .125 IN	164.00	SQFT		\$	
0450	06410		STEEL POST TYPE 1	100.00	LF		\$	
0460	06412		STEEL POST MILE MARKERS	70.00	EACH		\$	
0470	06417		FLEXIBLE DELINEATOR POST-W	1,075.00	EACH		\$	
0480	06418		FLEXIBLE DELINEATOR POST-Y	875.00	EACH		\$	
0490	06549		PAVE STRIPING-TEMP REM TAPE-B	189,250.00	LF		\$	
0500	06550		PAVE STRIPING-TEMP REM TAPE-W	378,500.00	LF		\$	
0510	06551		PAVE STRIPING-TEMP REM TAPE-Y	378,500.00	LF		\$	
0520	06568		PAVE MARKING-THERMO STOP BAR-24IN	310.00	LF		\$	
0530	06574		PAVE MARKING-THERMO CURV ARROW	22.00	EACH		\$	
0540	06575		PAVE MARKING-THERMO COMB ARROW	2.00	EACH		\$	
0550	06592		PAVEMENT MARKER TYPE V-B W/R	2,081.00	EACH		\$	
0560	06593		PAVEMENT MARKER TYPE V-B Y/R	437.00	EACH		\$	
0570	06600		REMOVE PAVEMENT MARKER TYPE V	2,518.00	EACH		\$	
0580	10020NS		FUEL ADJUSTMENT	64,959.00	DOLL	\$1.00	\$	\$64,959.00
0590	10030NS		ASPHALT ADJUSTMENT	114,470.00	DOLL	\$1.00	\$	\$114,470.00
0600	20191ED		OBJECT MARKER TY 3	48.00	EACH		\$	
0610	20366NN		REPLACE GRATE	38.00	EACH		\$	
0620	20411ED		LAW ENFORCEMENT OFFICER	2,000.00	HOUR		\$	
0630	20432ES112		REMOVE CRASH CUSHION	12.00	EACH		\$	
0640	21533EN		EMBANKMENT	355.00	CUYD		\$	
0650	21802EN		G/R STEEL W BEAM-S FACE (7 FT POST)	48,637.50	LF		\$	
0660	23143ED		KPDES PERMIT AND TEMP EROSION CONTROL	1.00	LS		\$	
0670	24189ER		DURABLE WATERBORNE MARKING-6 IN W	180,195.00	LF		\$	
0680	24190ER		DURABLE WATERBORNE MARKING-6 IN Y	142,311.00	LF		\$	
0690	24191ER		DURABLE WATERBORNE MARKING-12 IN W	5,008.00	LF		\$	

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PROPOSAL BID ITEMS

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Section: 0003 - DRAINAGE

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
0700	00461		CULVERT PIPE-15 IN	52.00	LF		\$	
0710	00462		CULVERT PIPE-18 IN	94.00	LF		\$	
0720	00464		CULVERT PIPE-24 IN	453.00	LF		\$	
0730	00466		CULVERT PIPE-30 IN	1,407.00	LF		\$	
0740	00472		CULVERT PIPE-60 IN	16.00	LF		\$	
0750	01010		NON-PERFORATED PIPE-4 IN	20.00	LF		\$	
0760	01020		PERF PIPE HEADWALL TY 1-4 IN	2.00	EACH		\$	
0770	01432		SLOPED BOX OUTLET TYPE 1-15 IN	2.00	EACH		\$	
0780	01450		S & F BOX INLET-OUTLET-18 IN	1.00	EACH		\$	
0790	01451		S & F BOX INLET-OUTLET-24 IN	2.00	EACH		\$	
0800	01480		CURB BOX INLET TYPE B	5.00	EACH		\$	
0810	01490		DROP BOX INLET TYPE 1	2.00	EACH		\$	
0820	01505		DROP BOX INLET TYPE 5B	1.00	EACH		\$	
0830	01691		FLUME INLET TYPE 2	4.00	EACH		\$	
0840	01756		MANHOLE TYPE A	1.00	EACH		\$	
0850	01767		MANHOLE TYPE C	1.00	EACH		\$	
0860	03262		CLEAN PIPE STRUCTURE	28.00	EACH		\$	
0870	08100		CONCRETE-CLASS A	1.86	CUYD		\$	
0880	08150		STEEL REINFORCEMENT	16.00	LB		\$	

Section: 0004 - BRIDGE-B00103L&R

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
0890	02483		CHANNEL LINING CLASS II	900.00	TON		\$	
0900	03294		EXPAN JOINT REPLACE 1 1/2 IN	57.00	LF		\$	
0910	03298		EXPAN JOINT REPLACE 4 IN	1,449.00	LF		\$	
0920	03299		ARMORED EDGE FOR CONCRETE	777.00	LF		\$	
0930	03304		BRIDGE OVERLAY APPROACH PAVEMENT	489.00	SQYD		\$	
0940	08019		CYCLOPEAN STONE RIP RAP	700.00	TON		\$	
0950	08020		CRUSHED AGGREGATE SLOPE PROT	70.00	TON		\$	
0960	08500		APPROACH SLAB	110.00	SQYD		\$	
0970	08504		EPOXY SAND SLURRY	402.00	SQYD		\$	
0980	08510		REM EPOXY BIT FOREIGN OVERLAY	4,878.00	SQYD		\$	
0990	08526		CONC CLASS M FULL DEPTH PATCH	8.00	CUYD		\$	
1000	08534		CONCRETE OVERLAY-LATEX	231.40	CUYD		\$	
1010	08549		BLAST CLEANING	6,316.00	SQYD		\$	
1020	08551		MACHINE PREP OF SLAB	1,438.00	SQYD		\$	
1030	21969NN		BEARING REPLACEMENT(ADDED: 11-14-12)	10.00	EACH		\$	
1040	21969NN		BEARING REPLACEMENT	5.00	EACH		\$	
1050	22146EN		CONCRETE PATCHING REPAIR(ADDED: 11-14-12)	50.00	SQFT		\$	
1060	22146EN		CONCRETE PATCHING REPAIR	560.00	SQFT		\$	
1070	23378EC		CONCRETE SEALING	36.00	SQFT		\$	
1080	23386EC		JOINT SEAL REPLACEMENT	377.00	LF		\$	
1090	24094EC		PARTIAL DEPTH PATCHING	46.00	CUYD		\$	
1100	24106EC		DECK SEALING	155,010.00	SQFT		\$	

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PROPOSAL BID ITEMS

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Report Date 11/14/12

Section: 0005 - TRAFFIC LOOPS

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
1110	04793		CONDUIT-1 1/4 IN	166.00	LF		\$	
1120	04795		CONDUIT-2 IN	60.00	LF		\$	
1130	04820		TRENCHING AND BACKFILLING	180.00	LF		\$	
1140	04829		PIEZOELECTRIC SENSOR	16.00	EACH		\$	
1150	04830		LOOP WIRE	4,500.00	LF		\$	
1160	04895		LOOP SAW SLOT AND FILL	1,183.00	LF		\$	
1170	20359NN		GALVANIZED STEEL CABINET	6.00	EACH		\$	
1180	20360ES818		WOOD POST	12.00	EACH		\$	
1190	20391NS835		ELECTRICAL JUNCTION BOX TYPE A	6.00	EACH		\$	

Section: 0006 - MOB AND DEMOB

LINE	BID CODE	ALT	DESCRIPTION	QUANTITY	UNIT	UNIT PRICE	FP	AMOUNT
1200	02568		MOBILIZATION	1.00	LS		\$	
1210	02569		DEMOBILIZATION	1.00	LS		\$	